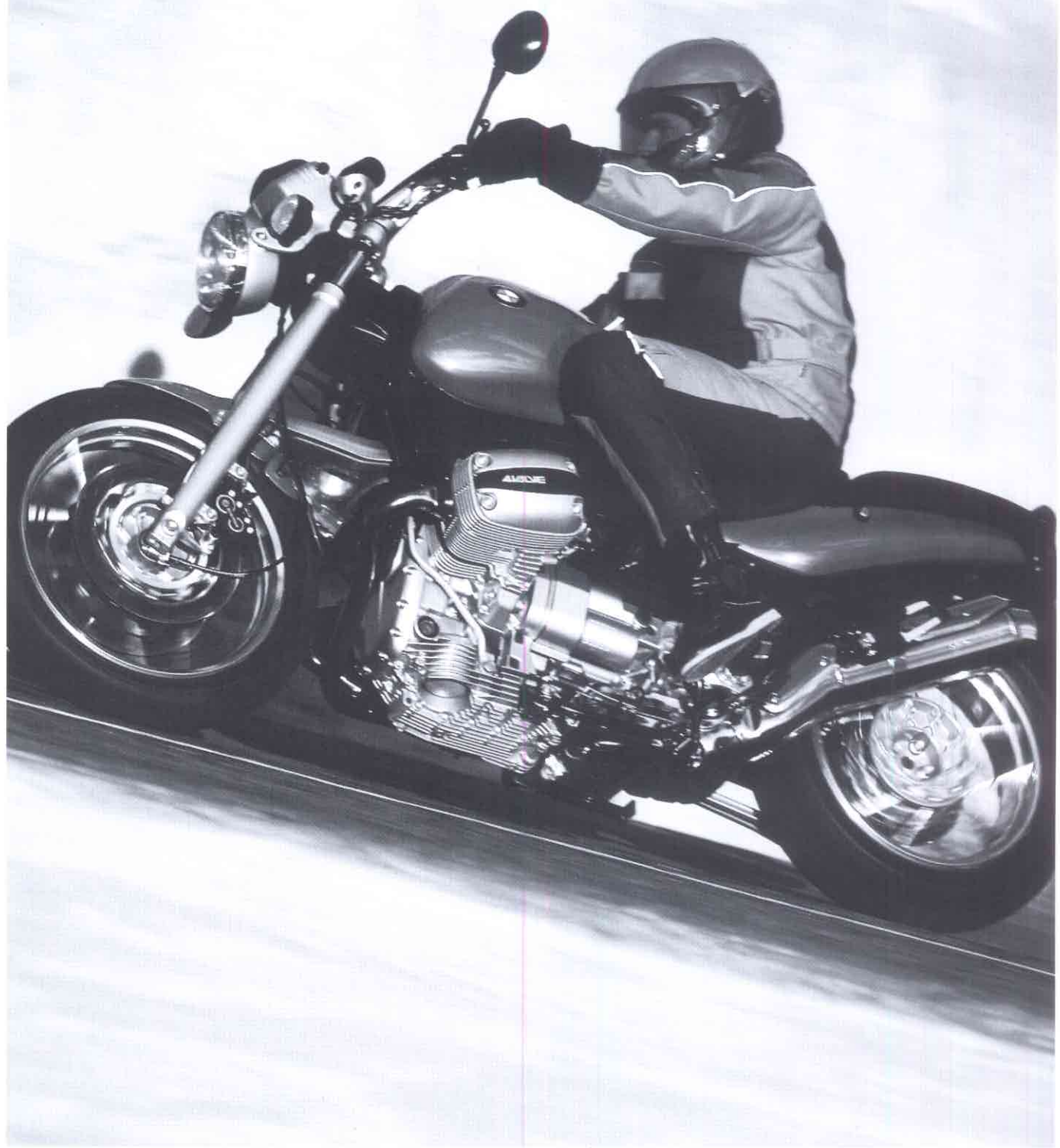


**The new
BMW Roadsters
R 850 R / R 1100 R**





The time is right for a completely new motorcycle concept.

Good ideas always turn up again.

Especially in motorcycle design. Motorcyclists in increasing numbers prefer to ride the way people did back in the early years of the motorcycle. At least according to the same basic principles – with plenty of fresh air in their face, on a “pure” motorcycle without a fairing. Top-quality engineering, clearly visible, classic lines and attractive details are back in.

But to be honest, is that all you require of your next motorcycle? We asked a number of riders for their opinion and discovered that they are not satisfied with what is around at the moment. As far as basic motorcycles are concerned, technology in particular is lagging behind and does not reflect what can be achieved today. As a result, riders looking for that original motorcycle feeling have to forfeit a great deal of the progress made recently.

Anything but a simple motorcycle.

It's true to say that everyone wants more. Riders who have just obtained their motorcycle licence, for example. It's obvious what they consider important: a motorcycle which is fun to ride immediately, easy to control and less physically demanding for the rider. It should of course be inexpensive both to buy and to run. But it shouldn't be too simple in design, or lose its value too quickly.

Riders with plenty of experience, on the other hand, are even less prepared to accept compromises. Not in terms of performance or comfort and in particular not when it comes to safer riding technologies. This means reserves for almost every situation which could be encountered in road traffic. Maximum versatility, of course – on all roads and for every destination, on extended tours and trips to the city centre. In short, a bike which is ideally suited to both men and women riders alike and looks genuinely attractive.

We had the idea:

a completely different motorcycle.

In other words, a totally new class of bike; a chance for the “basic” motorcycle to make the leap into the future that it deserves. One of our most important goals was for this motorcycle to be suitable for anyone who enjoys that original two-wheeler feeling, not just for specific groups of riders. We planned it for the experienced and the less experienced, for men and women riders. The result is worth a closer look.

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The new BMW Roadsters. R 850 R / R 1100 R

Pure motorcycling – in touch with the times.

And extremely well-prepared for the future.

Even before the first development stage, we knew: if the "naked bike" or basic motorcycle is to remain popular and make sense in the long term, then we can't afford to accept any more compromises: despite concentrating on essentials, our new model should not only satisfy the demand for unrestricted riding pleasure, but also the requirements of the world in which it exists. And these requirements are by no means what they used to be.

Traffic on our roads has become more and more congested. It therefore goes almost without saying that the "pure" motorcycle should be equipped with everything that makes the rider's task easier in risky situations – ABS, for example.

Environmental pollution is increasing all the time. It therefore makes sense to adopt the technology best suited to protecting the environment. More and more people are asking themselves whether a motorised two-wheeler should be allowed to produce more hazardous exhaust emissions than, say, an average mid-size car. Especially as many people ride motorcycles "just for fun". Anyone wishing to run a motorcycle these days with a clear conscience needs exhaust emission control by the latest catalytic converter technology, for instance.

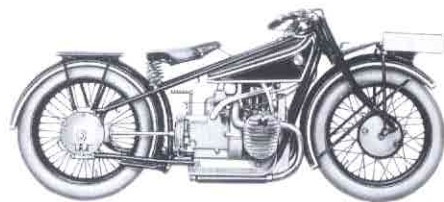
These two examples show what the new BMW Roadster is all about and why we have said farewell to nostalgic technical features, however attractive they might be. Not that this extremely modern motorcycle is ashamed of its origins – by no means! It has the unmistakable character that can only be acquired over a great many years.



A completely new motorcycle can also be a classic.

Especially if, like BMW, you stay loyal to established principles and develop them in line with modern requirements rather than chasing after the latest trends. There is no better example of this than the BMW Boxer, the unique flat-twin engine concept which has kept ahead of developments for over seventy years. Completely redesigned now, with state-of-the-art mechanical and electronic features and pro-environmental technology, the latest BMW Boxer engine took to the road in 1993 and caused an immediate sensation.

Not least because it has lost none of its classic virtues, such as its relaxed suitability for day-to-day riding. It was obvious to us that there couldn't be anything better for a "pure" motorcycle than technology with this unmistakable character and appeal.



70 years young: the BMW Boxer offers riding pleasure in its most elemental form.



BMW R 850 R / R 1100 R: Engine technology.

It's good to know you're starting from a sound basis.

For us, the Boxer was the only possible engine for the new BMW Roadster. The advantages which make this engine concept so popular were enough to make our minds up: the low centre of gravity, for example, which guarantees manoeuvrability or the ideal positioning of the cylinders – perfect for trouble-free cooling. The BMW Boxer is by its very nature a particularly smooth-running, high-torque engine, quite apart from its other practical advantages such as its easy-maintenance design and reliability.

Not surprisingly, the latest generation of BMW Boxers has already proved a great success in the BMW R 1100 RS and GS models. The 10,000th new BMW Boxer has been sold in just under two years – and received outstanding reviews from riders and specialist press alike.

Many people will be equally delighted to hear that there is now a second version of this successful engine – a newly developed 850 cc unit for all those looking for an inexpensive motorcycle suitable for first-time buyers.

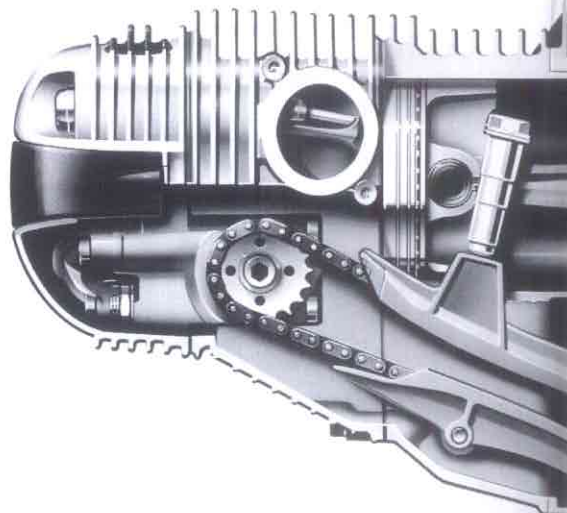
You want power reserves?

A maximum output of 59 kW (80 bhp) for the 1100 engine and 52 kW (70 bhp) for the newly developed 850 cc "open" version should be enough, especially as you're riding without a fairing. It will take you up to a top speed of 194 or 187 km/h (120 or 116 mph).

We have concentrated more on another quality: pulling power. This is something you can never have too much of – for rapid, safe overtaking for example. This is where the BMW Boxer's strengths really come to the fore. Take the 1100 cc engine, for example: maximum torque of 97 Nm as low as 5250 rpm, with more than 80 Nm almost across the entire speed range. In other words, sparkling acceleration whenever you need it or, to put it another way, full power even if you're riding at about 70 km/h (40 mph) in 5th gear, without judder or flat spots, as soon as you open the throttle. With genuine values like this, who could possibly need a roadster with more output?

You like high-tech? So do we.

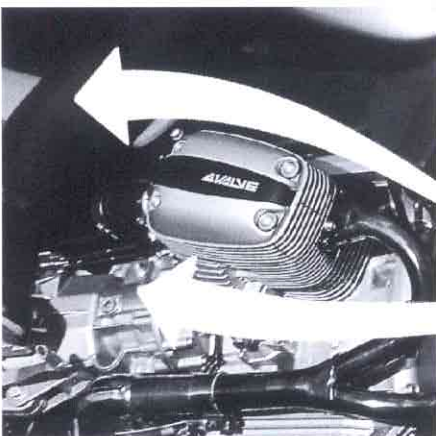
Here are a few gems for all those interested in technical concepts which you don't come across every day. Firstly, the engine has four valves per cylinder – ideal for maximum economy and well-balanced road behaviour. In order to achieve this, our engineers



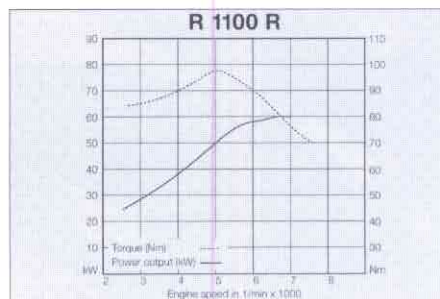
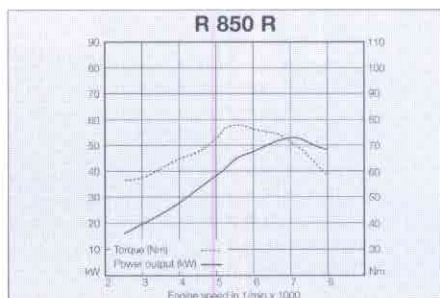
came up with a very unusual form of valve gear.

The diagram illustrates all the technical details of the "High Camshaft" principle, as it is called.

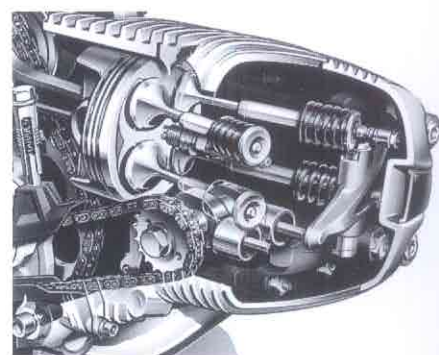
And its practical advantages? Extremely compact in overall width (the latest Boxer is narrower than its predecessor), low weight, particularly high strength and low noise level.



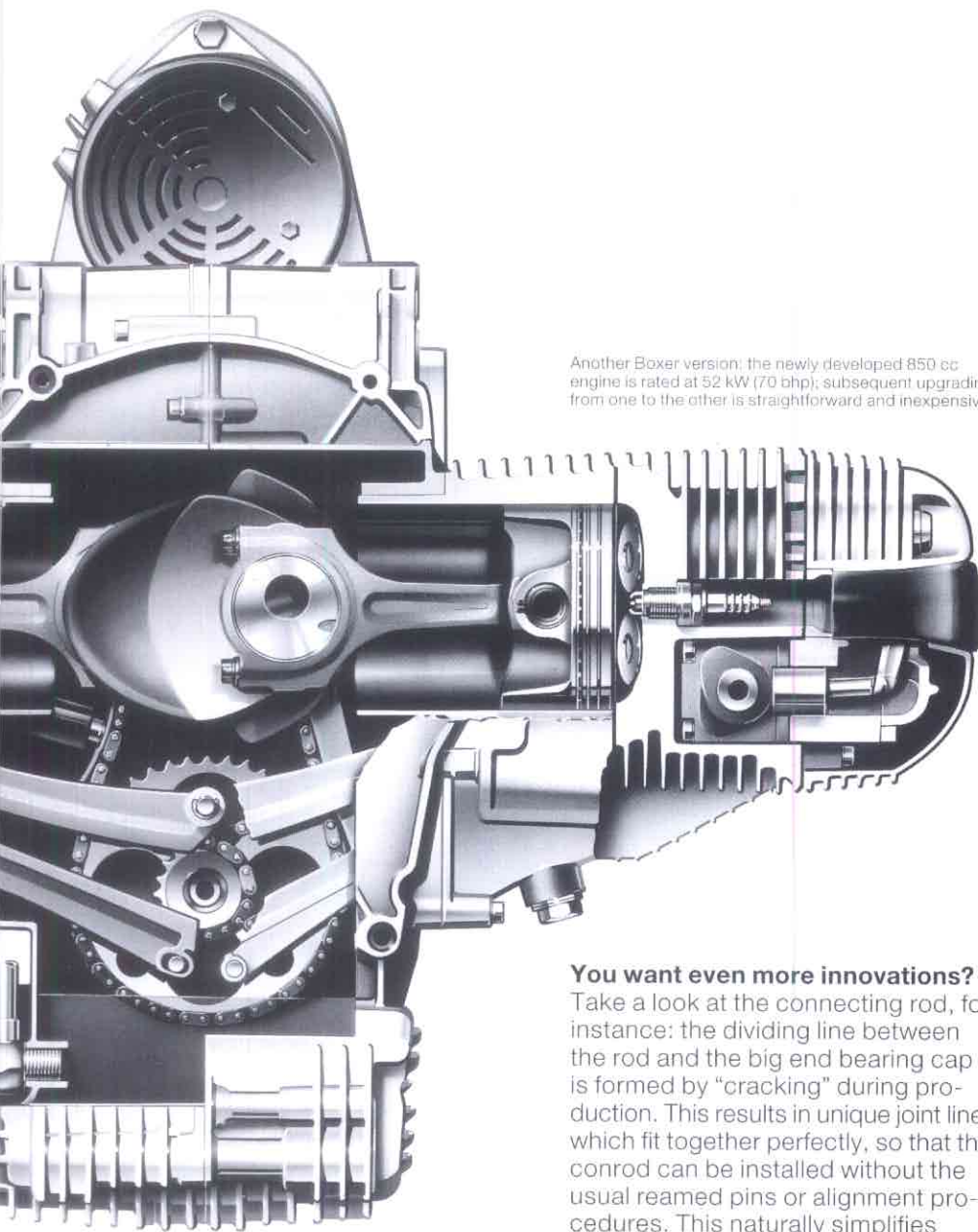
Boxer virtue: the protruding cylinders are ideally cooled in the slipstream, at the same time protecting the rider against accident injury.



Striking curves – torque is impressive from all versions.



A closer look: HC valve gear with timing chain, side camshaft, tappets/push rods and the rocker arms which actuate two of the four valves.



Another Boxer version: the newly developed 850 cc engine is rated at 52 kW (70 bhp); subsequent upgrading from one to the other is straightforward and inexpensive.

Long live the Boxer.

This has always been our motto, and will remain so. Durability certainly doesn't suffer, despite high performance. This is obvious from the fact that the bike only needs a routine service every 10,000 km (6,500 miles) – a real advantage with regard to running costs.

Just one progressive step among many: the Boxer's ingenious combined air/oil cooling system. Oil is supplied to the area around the exhaust valves, where high thermal loads are encountered, via a separate cooling circuit with two integrated oil coolers. Together with the cooling effect of the slipstream, the oil helps to keep engine temperatures down.

Quiet please!

We've paid particular attention to reducing mechanical vibration and therefore engine noise level in general. We have retained the classic Boxer sound, but made it much more subtle.

A combination of numerous minor details is what makes the BMW Boxer so quiet – the valve gear or the new conrods, for example, plus the cast light-alloy pistons which develop very low forces of inertia and thus counteract possible vibration. The cooling fins too suppress vibration and natural frequencies effectively. The Boxer is not only smooth-running, but also already complies with noise emission limits not yet due for enforcement.

You want even more innovations?

Take a look at the connecting rod, for instance: the dividing line between the rod and the big end bearing cap is formed by "cracking" during production. This results in unique joint lines which fit together perfectly, so that the conrod can be installed without the usual reamed pins or alignment procedures. This naturally simplifies repairs to the engine if they should become necessary.



"Cracked" conrods – another new contribution to light-weight design. Lower weight means greater refinement and a reduction in vibration.

BMW R 850 R / R 1100 R: Engine electronics and environmental technology.

The future lies in electronics.

After all, they are the best and probably the only way to meet increasing engine development demands. Especially if the mechanical features are already as perfect as they are on the BMW Boxer.

The problems which development engineers currently face become obvious if we take a look at the conflicting requirements they are expected to satisfy.

On the one hand, engines are expected to have substantial power reserves and a balanced, harmonious character. On the other hand, they are supposed to be as economical as possible and perform their task without imposing a burden on the environment. And despite all this, they are expected to withstand high, continuous loads.

Plenty of good reasons for us to bring the future forward and become the first motorcycle manufacturer to use an ultra-modern electronic engine management system. This system has more than proved its worth – especially on the BMW Boxer. Find out why for yourself.

Motronic brings intelligence into play.

In simple terms, the Digital Motor Electronics system, as used on BMW's Boxer engines, consists of an advanced microcomputer which monitors and controls various engine functions. It ensures that the engine operates perfectly in all conditions, in other words, more efficiently than with conventional carburettors and ignition systems. The practical consequence is clear for all to see: lower fuel consumption and fewer exhaust emissions, despite the same power output.

In addition, and this is probably the most powerful argument in favour of Motronic, it is ideal in combination with a closed-loop three-way catalytic converter – currently the most effective means of exhaust emission control.

And something else speaks in favour of this system: it maintains outstanding reliability throughout the motorcycle's entire operating life. Problems such as wear, contamination and faulty maintenance are as good

as ruled out – in contrast to conventional systems. Even in the extremely unlikely event of one of the Motronic components failing, its emergency-run characteristics usually allow you to continue the journey. An integrated fault memory makes sure that faults can be traced and rectified quickly and therefore inexpensively.



Motronic is maintenance-free and, using the diagnosis system, you can check in a matter of minutes at the BMW workshop whether it is functioning perfectly.

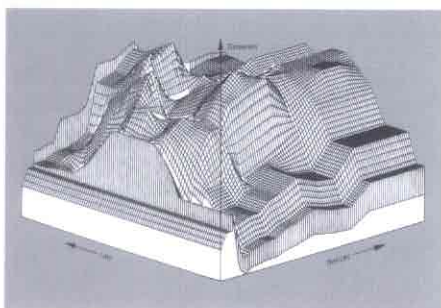


You want the exact details?

The principle behind the Motronic is that it combines all engine functions in a self-contained control system. Via a number of external sensors, it is kept informed of all operating conditions, e.g. throttle position, engine speed, air temperature and pressure, oil temperature and the measured values from the oxygen probe if a catalytic converter is fitted.

Using these data and with the help of a pre-programmed ("mapped") characteristic (see diagram), the computer calculates the most effective ignition timing and exactly how much fuel should be injected. This all happens in just a few fractions of a second and is repeated for each combustion cycle. In practical terms, its task is to cut fuel consumption to the practicable minimum.

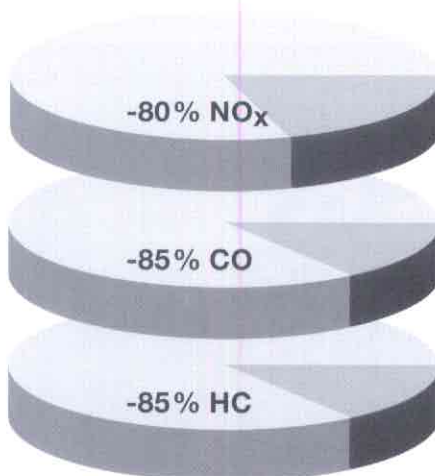
However, the Motronic is capable of even more. For example, it improves engine response when you open the throttle suddenly, and the power curve is smoothed out. Last but not least, the Motronic's fuel cutoff when coasting saves fuel even if engine speed drops below 2000 rpm.



Using this mapped characteristic, the Motronic determines the perfect combination of ignition timing and the duration and amount of fuel injected.

A major plus for the environment: BMW's catalytic converter technology.

The BMW Boxer's advanced engine management system is environmentally friendly by any standards. Lower fuel consumption automatically means fewer emissions, after all. But that's no longer enough today, not to mention in the future. The best and currently the most effective concept at the moment is the three-way catalytic converter with oxygen sensor. As the graph shows, the result is certainly impressive: approx. 80 % fewer total pollutants.



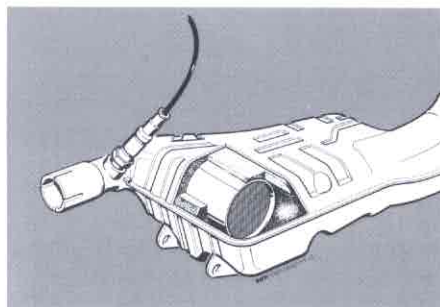
A worthwhile choice – the three-way catalytic converter, available as an optional extra, controls exhaust emissions extremely effectively (results according to ECE R 40 test).

Catalytic converter and Motronic – a perfect team.

To make sure that as many pollutants as possible are broken down inside the catalytic converter, air and fuel must be mixed at a constant ratio of 14:1 before combustion. Even slight deviations from this ideal value reduce efficiency.

This is the purpose of lambda control. A probe in the exhaust gas flow monitors the mixture ratio continuously and transmits this data to the Motronic. Since precision and reaction speed are two of the greatest strengths of the Digital Motor Electronics, the air and fuel can be metered extremely accurately and the catalytic converter then operates at maximum efficiency.

Incidentally, BMW R 850 R / R 1100 R models use a high-grade metal catalytic converter noted for its unusually long operating life and durability. It does not noticeably affect the Boxer engine's fuel consumption or power output. The catalytic converter is available as an optional extra.



The three-way catalytic converter is located inside the silencer at a point where the flow of exhaust gas strikes it directly; together with the Motronic, the oxygen sensor in front of it guarantees a high level of efficiency.

BMW R 850 R / R 1100 R: Suspension technology.

Suspension is what makes a bike fun to ride – look at the BMW Roadsters.

Our aim: to make the new BMW Roadster as manoeuvrable and uncomplicated as possible. It should handle predictably in all situations, be comfortable even on poor road surfaces and, at the same time, transfer its power to the road as safely as possible. In other words, a motorcycle you can trust and which you can ride at higher speeds and heeled over to steeper angles. Have we promised too much? Evidently not, if you look at the results. The new Roadster's road behaviour and handling represent an outstanding achievement in the "unfaired bike" class, say our engineers. And of course, this

A stroke of genius: the BMW Telelever.

Anyone who studies motorcycle design will know that the telescopic fork has been with us for many years, despite not being the ideal solution. Many engineers have looked for a new, superior alternative. This was the case at BMW. The result of its intensive research was the BMW Telelever, a front suspension layout which combines the advantages of the telescopic fork with those of a front swinging arm. This concept was the most convincing, and clearly not only for us: the test team at the "Motorrad" magazine (18/93), who compared it with existing alternatives, was also impressed.

Another important point is that castor and wheelbase are kept largely constant throughout the entire range of suspension travel, even when severe loads are applied – which means excellent directional stability in all situations. Effective anti-dive geometry when braking hard is also guaranteed. As a result, sufficient residual spring travel remains – with the associated comfort and safety benefits.

The third advantage is that very little friction can build up in the fork, which reacts quickly and extremely sensitively as a result. This greatly increases ride comfort, especially on bumpy roads.



The BMW Telelever – a new benchmark for all telescopic forks.

suspension has already been through its baptism of fire and received enthusiastic praise in the press when introduced on the BMW R 1100 RS and GS.

Built for the future.

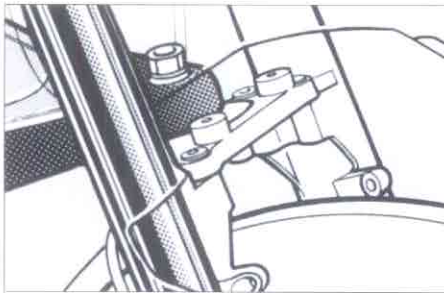
At first glance you'll notice that the BMW Telelever also has fork tubes, in fact much of the conventional motorcycle look has been retained. The difference is that they contain no springs or dampers, for these functions are performed by a single, central suspension strut. Another new feature is the large leading link which is mounted on the engine block and attached by a ball joint to the bridge between the lower fork tubes. The upper fork tubes are pivot-mounted to the top of the frame by means of a ball joint.

This arrangement, although not unduly complex, has a number of technical advantages. Braking force, for instance, is directed through the leading link directly into the massive engine block – in other words, the fork is extremely torsionally rigid.

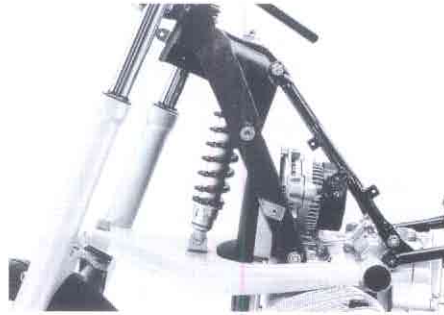
To enhance comfort still further, the Telelever on the BMW R 850 R and R 1100 R models has also been fitted with a steering damper and an "anti-tilt" device. This prevents the rider from sensing up and down movements of the suspension at the handlebar.

Built to last.

It goes without saying that the BMW Telelever is extremely stable and strong, without the disadvantage of additional weight. It is even suitable for extreme off-road use – take a look at the BMW GS! However, the really pleasing thing about the Telelever is that the whole system is so straightforward in design and almost entirely resistant to wear. It can cover around 100,000 km (60,000 miles) without maintenance, and even the damper oil does not have to be changed. Typical telescopic fork problems, play at the steering head bearing for example, are ruled out from the start.



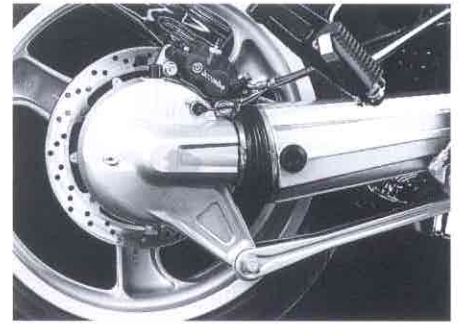
Zero-play ball joints, tried and tested on cars for many years, make the BMW Telelever extremely resistant to wear.



BMW Paralever: shaft drive with no regrets.

A more effective rear wheel suspension concept is hard to imagine: shaft drive installed in a single massive swinging arm, which keeps going for thousands of miles, requires only minimum maintenance and, on top of all this, makes removing the wheel child's play.

But that does not even exhaust the special features of the BMW Paralever. Its principle enables us to reduce disturbing influences of the sort which can occur with shaft drive. You won't feel this suspension stiffen up when decelerating on corners. On the contrary, you will soon come to appreciate your bike's impeccable, trouble-free and extremely safe handling.

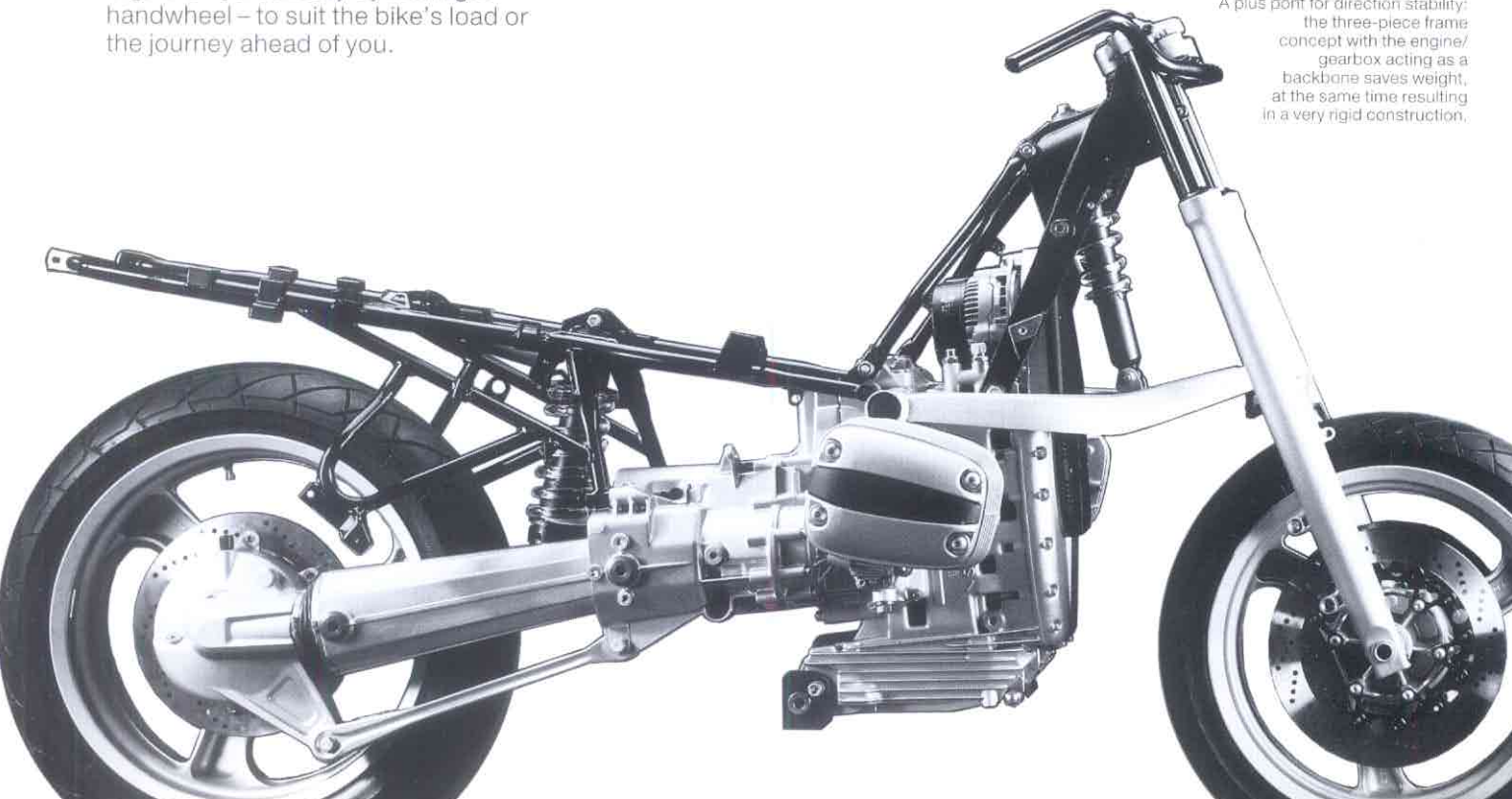


The key to the BMW Paralever's impeccable handling is that it behaves like an extremely long swinging arm. The central spring strut can be pre-loaded to suit individual requirements.

A new spring in your step:

The BMW Roadster has top-quality suspension elements: a modern central strut with coil spring and single-tube gas-filled damper is used at front and rear. Rebound damping is continuously adjustable at the rear, spring pre-load is also extremely easy to regulate hydraulically by turning a handwheel – to suit the bike's load or the journey ahead of you.

A plus point for direction stability: the three-piece frame concept with the engine/gearbox acting as a backbone saves weight, at the same time resulting in a very rigid construction.

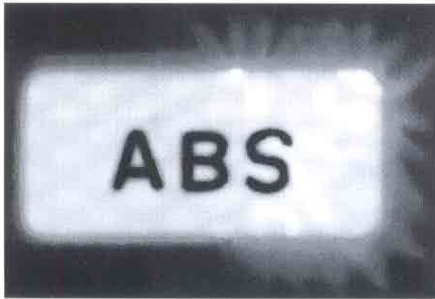


BMW R 850 R / R 1100 R: Brake technology and ABS.

We want you to feel safe when you ride.

As safe as technology can make motorcycling. Our electronically controlled ABS for motorcycles was probably the most important and most revolutionary development ever made in this direction. It enables the rider to be able to brake hard without problems in critical situations. Even on wet roads or in other conditions which are hard for riders to judge.

Only ABS can prevent what can happen to even the finest riders – in a



Having to brake hard in weather like this? A worrying thought, unless your bike has ABS.



panic situation, the rider slams on the brakes far too quickly, causing the motorcycle's wheels to lock. The bike becomes unstable and goes out of control – a crash is the inevitable consequence.

Not surprisingly, around 50,000 BMW riders have already opted for the BMW ABS option. Our aim was always to offer this safety feature not only for a single top model, but on the entire model range – sports tourers, enduros and, of course, our new BMW Roadster. No other motorcycle in this class is currently available with ABS.

BMW ABS: simply outstanding.

It is to our credit that despite our lead in ABS matters, we weren't content to sit back and let the others catch up. We carried on improving this technology – and the result is certainly something to be proud of. BMW's ABS now works so efficiently that even our experienced test riders are rarely able to outperform it using conventional brakes. The superiority of ABS only really comes to the fore on slippery and unpredictable road surfaces. Even braking hard on snow and ice doesn't cause it any problems – even in situations like this, ABS does not waste any braking distance at all.

ABS also keeps you on the safe side if, for example, the rear wheel lifts off the ground when decelerating rapidly. The system's advanced software is even able to detect and cope with exceptional braking conditions such as these.

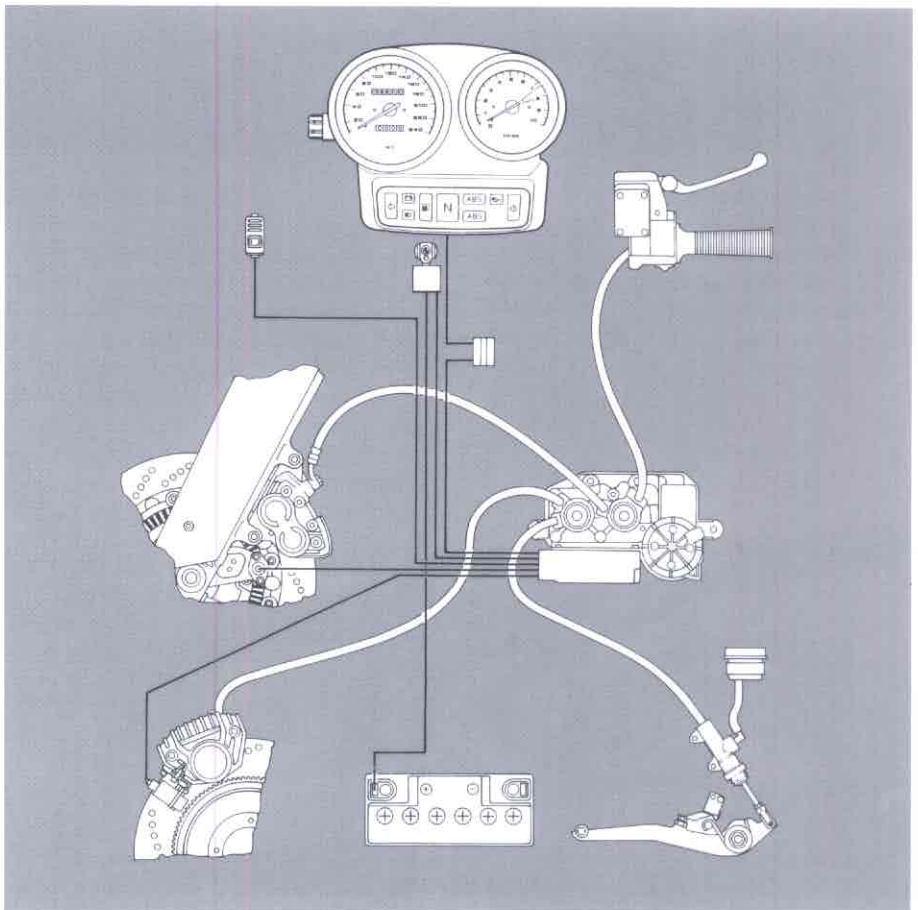
ABS to the rescue, in a fraction of a second.

Our ABS acts and reacts extremely quickly, whenever one of the motorcycle's wheels starts to lock after being braked suddenly.

How it functions: sensors monitor rotation of the wheels continuously and pass information on to the system's microcomputer. As soon as the tendency for a wheel to lock is detected, the pressure modulator is activated. Its task is to reduce the braking pressure applied by the rider until the wheel starts to turn again, but at the same time to maintain maximum braking force at the wheel. ABS of course acts separately on the front and rear wheels.

The pressure modulator, with its very advanced electronic system and software, is the key to BMW's motorcycle ABS: in order to regulate braking pressure very precisely, very quickly and sensitively, it has a moving piston which can react amazingly rapidly: in only 4/1000ths of a second! The pressure ratios are varied so precisely

The pressure modulator plus control unit, the key to the ABS, is very compact and light in weight, despite the immense additional safety it provides.

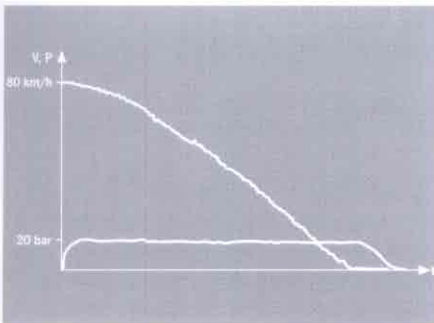


that each wheel can be braked as closely as possible to the perfect speed, just below that at which it locks completely. In practical terms, ABS does not waste a scrap of the available braking distance.

If you are now asking yourself how the motorcycle comes to a halt, for instance at traffic lights, then rest assured – ABS simply switches off at speeds below 2 kph.

BMW ABS: the brilliant all-rounder.

One pleasant result of our very elaborate concept is that you don't feel the actual control process – braking is extremely smooth and comfortable, without judder or other unwelcome side-effects. In addition, you can rely one hundred percent on the system



BMW's motorcycle ABS keeps braking forces at a very high level throughout the full brake application. This is done much more accurately and, above all, more safely than any rider would normally be capable of in a panic situation.

functioning safely – ABS has been tried and tested in practice over countless miles.

It's also good to know that its electronics meet the highest possible standards. ABS operates with three independent computers which monitor each other permanently. If one of them detects a malfunction, ABS shuts down automatically and the fault is indicated by a telltale. But this doesn't cause any major problems – the brakes retain their full, highly impressive operating efficiency.

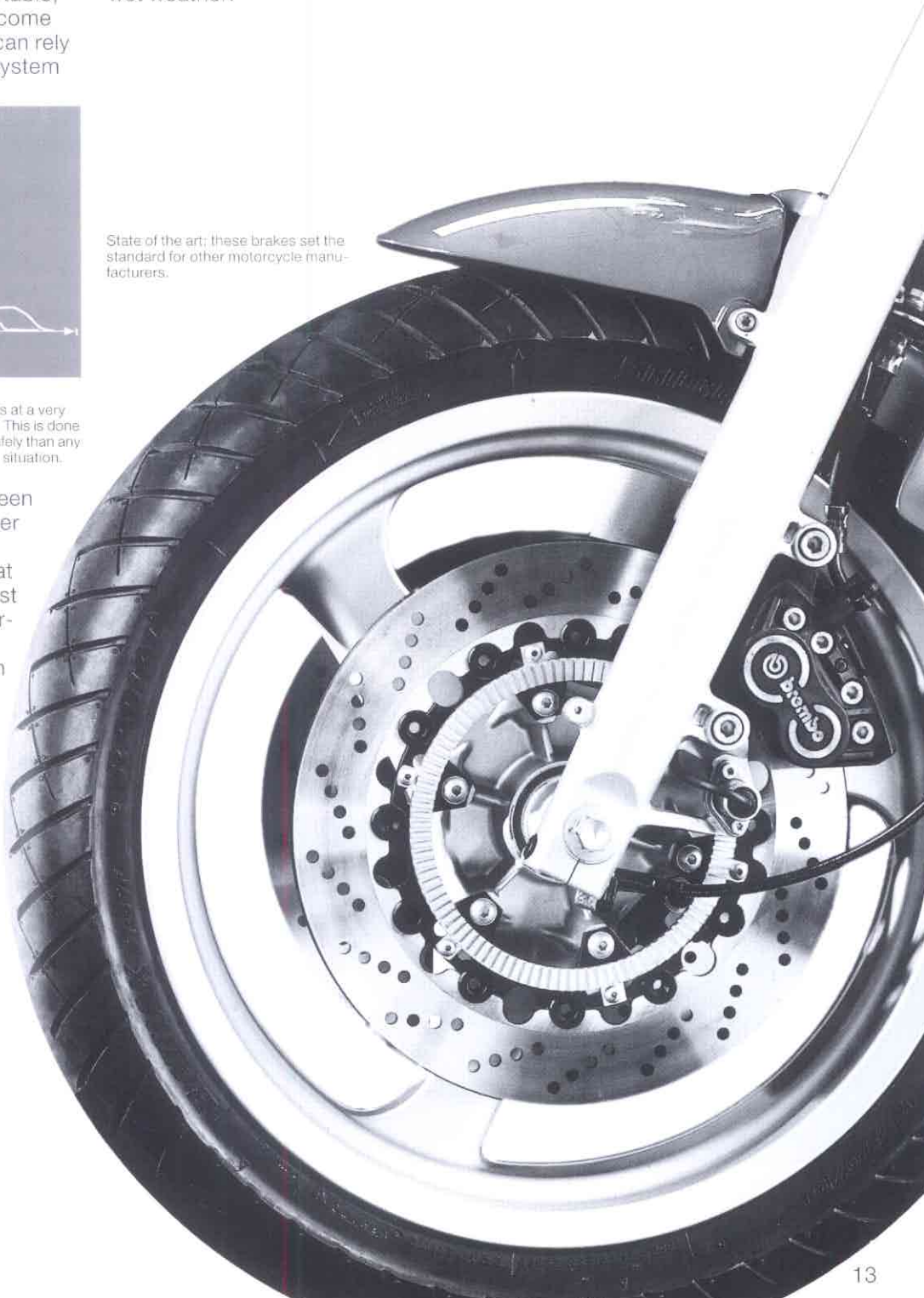
Brake technology?

You can't get enough of it.

We believe that, when it comes to brakes, compromises are unacceptable – no matter whether we are talking about large touring bikes or the "pure motorcycle". The brakes on the BMW R 850 R / R 1100 R certainly lack nothing. The front wheel has twin floating 305 mm dia. stainless-steel discs with 4-piston fixed calipers and angular brake pad wear compensation. The rear wheel has a 2-piston floating caliper and a 276 mm dia. stainless-steel disc. The brake system remains fully effective even in wet weather.

What does all this really mean? Quite simply – top quality. The brakes are not only extremely effective but also very sensitive in their response to the pedal or lever. They also cope easily with extreme continuous loads, for example when riding down steep and lengthy mountain passes.

State of the art: these brakes set the standard for other motorcycle manufacturers.



BMW R 850 R / R 1100 R: Comfort, ergonomics and equipment.

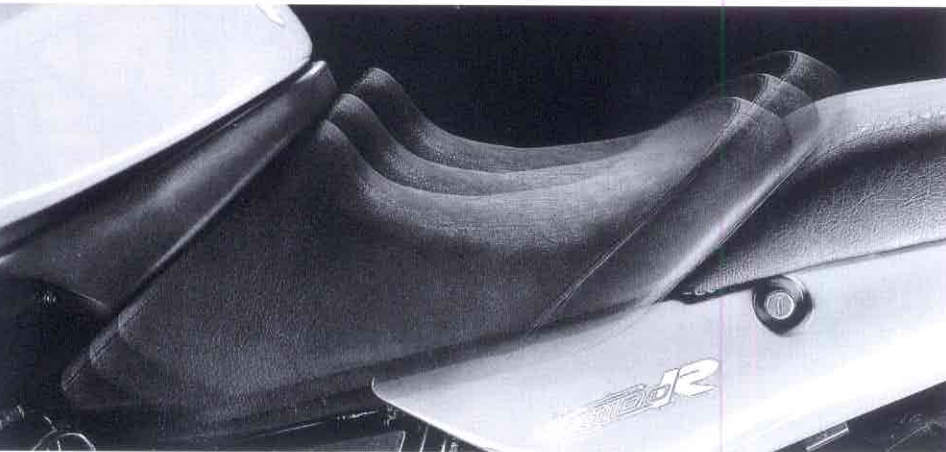
At last, something for every rider.

Be honest, what's the point of the perfect technology if you don't feel one hundred percent at home on your motorcycle? If comfort leaves something to be desired and your body soon starts complaining about how far you have ridden? A bike on which you can relax day in, day out, should be perfectly matched to you as the rider and genuinely satisfy all your requirements.

Is this possible? With the new Roadster's "individual ergonomics", we believe it is. The standard rider's seat can be adjusted in height from 760 to 780 or 800 mm. No matter how tall you are, you can easily find your ideal seated position.

Not only can you put both feet comfortably on the ground when you stop, you will also be able to sit upright behind the comparatively wide, high handlebar with anti-tilt device. And the ergonomically designed seat provides excellent support when accelerating and braking.

An adjustable-height rider's seat – not available on any other basic motorcycle model.



Do you sometimes go on longer journeys?

Why not – even if you have chosen to ride a model without a fairing, nothing should stand in your way. Take the seat upholstery, for instance. This has always been one of the special strengths of BMW motorcycles.

Not to mention the excellent seated comfort for pillion passengers. You certainly won't have any trouble clocking up 300 kilometres (over 150 miles) without a break, particularly since the 21-litre tank holds enough fuel for a journey of at least this length.

The BMW Roadster's high payload is also suitable for long tours – as much as 215 kg for a bike with standard equipment and a full tank of fuel.

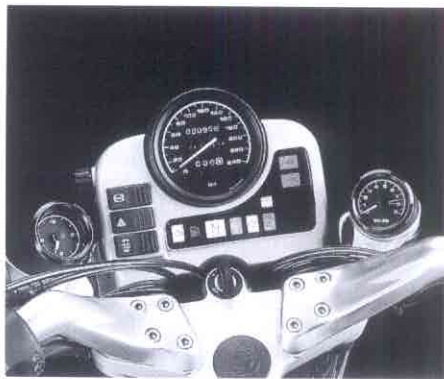
And since you may well find yourself riding at night, we've fitted the BMW Roadster with an extremely efficient headlight – something you can't always take for granted on other motorcycles.



Elegantly shaped: the new BMW sheet-steel tank holds a generous 21 litres of fuel.

It's the details that count.

The little extras "on the side" so often contribute most to the fun you derive from your motorcycle. Details which you perhaps haven't specifically looked for, but which you appreciate all the more when you have them. For example, a 12 Volt power socket or the one-key system for all the motor-



Attention to detail from BMW: the same key for all locks, including ignition, pannier cases and topcase.

cycle's locks – incidentally, you can also lock the topcase and pannier cases with this "ignition key". And no motorcycle should be without a hazard warning system or a side stand with starter interlock device to prevent you from riding off with the side stand still down. And if you should be obliged to take matters into your own hands out on the road, then your BMW R 850 R or R 1100 R won't leave you in the lurch: with the extensive high-quality toolkit, including a puncture repair kit, you can achieve a great deal.

How to turn a motorcycle into your motorcycle.

With the numerous optional extras we offer, it's no problem. We have always catered for our riders' special requirements. In this way, you can be sure that accessories are perfectly matched to your motorcycle and that when it comes to quality, you don't have to accept any compromises. Who could offer you a better guarantee?

Your BMW R 850 R or R 1100 R is available with heated handlebar grips and additional instruments (rev counter and clock), or you can give it a personal "classic" touch with the help of BMW cross-spoke wheels, a black seat or a driveline "all in black". Anyone planning a major tour will find just the equipment he or she is looking for – windshield, tank bag or the BMW system panniers, which are extremely easy to handle. There is even a special alarm system, plus a cylinder guard, knee pads and foam grips for even greater comfort.

Attractive, lasting values.

When we developed the new BMW Roadster, we naturally attached a great deal of importance to its appearance. After all, a motorcycle with such outstanding technology should also set high visual design standards.

At BMW, appearance is never an end in itself, but always combined with practical function. One example among many: the high-quality chromium-plated silencer. Under its surface you will find stainless steel, the most durable material for this purpose.

In other words, "inner values" are a top priority at BMW and can be interpreted quite literally, for example, as "long-term durability, even under extreme loads". Or how about "low fuel consumption" or "concepts for easy servicing and repair work which save you money". Anyone familiar with BMW will also know that typical accident-repair and other replacement parts are very reasonably priced – independent tests in specialist

publications confirm this time and time again. If you turn the pages of such magazines, you will usually find another of BMW's "value-for-money" advantages a bit farther on. The "used motorcycle" section is almost always evidence of the high resale value of BMW motorcycles.



Thinking ahead: BMW system panniers were developed in the wind tunnel to make sure they have no negative effect on the bike's handling. Particularly ingenious: the new multifunctional handle makes the case considerably easier to carry and use.

BMW motorcycles: the whole world of riding.

You can never have too much of a good thing:

Especially if you look to the future. This is why so many people familiar with the motorcycle market are impressed time and time again by the trouble that BMW goes to on behalf of its customers. And why BMW riders are loyal to the brand.

This is certainly not only due to the quality of our motorcycles. Many consider the world we have created for BMW riders, which offers everything you could ever need for your motorcycling, equally as important. It is essential to take this worthwhile standard of partnership into account before deciding which motorcycle to buy.

Wherever you are in the world, this welcoming sign isn't far away.

Some examples?

Let's start with Service: there are numerous Authorised Dealer service outlets in more than 100 countries around the world. You can always be sure of a truly high standard of quality, including rapid and reliable spare parts supply, if you are looking for advice or bring your bike into the workshop. We hardly need be surprised any more when BMW dealers achieve above-average results in tests conducted by the specialist media.

But don't forget – your BMW dealer is not only there to back you up, but is also the right person to consult for advice on financial matters. Your dealer can give you an attractive quote for BMW finance and leasing – for new or used BMW motorcycles and even for buying accessories and rider's clothing.



BMW rider equipment – yet another special feature.

You may be surprised to hear that we take a serious interest in rider equipment. And the results are certainly something to be proud of – we have created a complete range of equipment for every type of rider: for men and women, in classic leather or modern materials – or both at the same time. Take the BMW Atlantis leather suit: thanks to its GORE-TEX® membrane, it remains watertight in even the heaviest of downpours – the world's first suit of its kind. Then there's



The BMW two-piece Roadster leather suit offers top quality and a high standard of safety.



the Active Line range of versatile sportswear and adventure fashion: ideal for all those who, in addition to motorcycling, pursue other sports activities.

It's easy to get started:

With our dealer training programmes for inexperienced motorcyclists and riders who haven't been on a motorbike for a long time, but wish to get back into the routine gradually. An exciting weekend tour is just the answer – see the BMW travel programme* for offers. Or perhaps you'd prefer to go on holiday with like-minded motorcycle enthusiasts.

Ask your BMW dealer for details of local activities – or maybe some a little further afield.

Any questions?

Your BMW motorcycle dealer will be pleased to provide further information on anything you wish to know – just as you would expect from a partner dedicated to serving your best interests. Or why not have further information sent to you – rider and motorcycle equipment catalogues, details of BMW's travel programme, information on other BMW motorcycles or, most recently, on our BMW Boxer Classic programme. Write and tell us where your special interests lie.

* not in the USA



Please contact your motorcycle dealer for further information about rider and motorcycle equipment catalogues, BMW motorcycles or our BMW Boxer Classic programme.



BMW R 850 R and R 1100 R Roadsters. The time is right to ride one yourself.

You've had a good look around at what other manufacturers are offering before reading everything there is to know about the new BMW Roadster. Just as well, for you are surely no longer in any doubt as to which motorcycle concept you should choose. When it comes to the concept we have to offer in the "unfaired" category, we have a number of aces up our sleeve. ABS, catalytic converter, variable ergonomics and state-of-the-art suspension technology – there have never been better arguments in favour of an "unfaired bike". Especially as it offers everything else you need to guarantee riding pleasure – from the powerful, down-to-earth sound of the BMW Boxer to its outstanding everyday qualities.

Do you want to find out right now? You are warmly invited to test ride the BMW R 1100 R or R 850 R at length. Nothing will convince you more.



Engine:

2-cylinder, 4-stroke flat twin (Boxer), with 4 valves per cylinder
 Displacement: 1085 [64.8"] cc
 Bore x stroke: 99 [87.8"] (3.9" [3.5"*)] x 70.5 mm (2.8")
 Output:
 59 [52"] kW (80 [70"] bhp) (DIN) at 6750 [7000"]/min
 Torque:
 97 [77"] Nm (71.6 [56.8"] lb/ft) at 5250 [5500"]/min
 Compression ratio: 10.3:1
 Cooling: air/oil
 Valve gear: HC, chain-driven
 Mixture preparation: Bosch Motronic MA 2.2 with fuel cutoff when coasting
 Clutch: single dry plate
 Gearbox: 5 ratios
 I: 4.16; II: 2.91; III: 2.13; IV: 1.74; V: 1.45:1
 Final drive ratio: 3.0:1 [3.6:1*]

Electrics:

Engine management system: Motronic
 Alternator: 700 Watt
 Battery: 12 Volt, 19 Amp/h

Dimensions and weights:

Fuel tank capacity: 21 l (5.5 US gal/4.6 Imp gal), including 4 l (1.1 US gal/0.9 Imp gal) reserve
 Seat height (at unladen weight):
 780, 780 or 800 mm (29.9", 30.7", 31.5")
 Unladen weight, ready for road:
 235 kg (518 lb)
 Gross weight limit:
 450 kg (992 lb)

Performance and consumption:

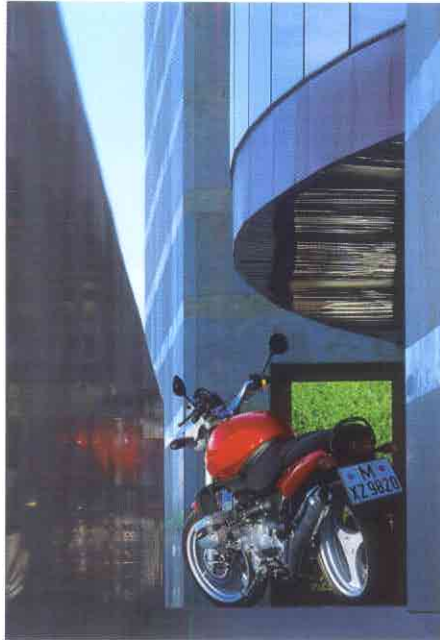
Top speed:
 app. 194 [187"] km/h (120.6 [116.2"] mph)
 Flexibility, 60 - 140 km/h (37 - 87 mph): 9.9 [13.0"] s
 Fuel: super (premium) grade, unleaded

Frame and suspension:

Frame: 3-piece, comprising front and rear sections and load-bearing engine
 Front suspension: BMW Telelever with central suspension strut,
 Travel: 120 mm (4.7")
 Rear: single swinging arm (BMW Paralever)
 Drive: shaft
 Suspension strut: central, with coil spring and single-tube gas-filled shock absorber;
 7 spring preload settings,
 continuously variable shock absorber rebound rate
 Travel: 135 mm (5.3")
 Front brake: twin 305 mm (12.0") dia. floating discs;
 4-piston fixed calipers with angular pad wear compensation
 Rear brake: single 276 mm (10.9") dia. disc, 2-piston floating caliper
 Brake pads: resistant to wet fading
 Wheels: cast alloy
 Front tyre: 120/70 H ZR 17, low aspect ratio (tubeless)
 Rear tyre: 160/60 H ZR 18 (tubeless)

*Values in square brackets [] apply to R 850 R; this model is not available in Australia





The models illustrated and the technical data are for the versions and equipment sold in Germany. They may include certain special equipment which is not part of the standard specification. Due to varying national legislation, specifications in other countries may differ from the models or equipment illustrated here. For information on the precise equipment specification, please contact your importer or dealer. Subject to amendments in design and equipment.

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This catalogue has been printed on 100 % chlorine-free bleached paper.
Printed in Germany 9/94

411 200 720/UX-V-10